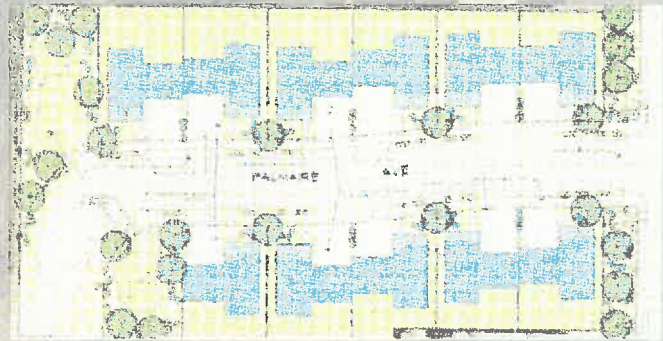




Palmaire Courts

An 11 Lot R1-4 PRD Residential Subdivision

Located At 7136 North 54th Avenue, Glendale Arizona



Planned Residential Development (PRD) Booklet

GPA08-06

ZON08-20

Submitted September 10th, 2008

Resubmitted November 4, 2008

NOV - 4 2008

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DEVELOPMENT TEAM:

Property Owner/Developer:

Habitat for Humanity of Central Arizona
9133 Northwest Grand Ave
Peoria, AZ 85345



Architect:

Reset Studios
407 West Osborn
Suite 200
Phoenix, Arizona 85013



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PRD REQUEST SUMMARY:

This is a PRD approval request for an approximately 1.42 acre infill development site to permit the construction of eleven (11) single-family homes. These homes are to be developed by Habitat for Humanity of Central Arizona, a non-profit organization with years of development experience. Due to unique site conditions, minor deviation from standard R1-4 Zoning District development standards is required to permit feasible development of the subject site. This PRD narrative is respectfully submitted to document the proposed standards and their justification, as required by Section 6.205 of the City of Glendale Zoning Ordinance.



6.202.C.1 - Legal Description

PARCEL NO. 1:

The South half of the East 2/3rds of Lot 5, Block 2, PARK PLACE, according to Book 4 of Maps, page 40, records of Maricopa County, Arizona;

EXCEPT the South 25 feet thereof.

PARCEL NO. 2:

The South half of the West 1/3rd of Lot 5, Block 2, PARK PLACE, according to Book 4 of Maps, page 40, records of Maricopa County, Arizona;

EXCEPT the South 30 feet thereof.

PARCEL NO. 3:

The South half of the East 1/3rd of Lot 6, Block 2, PARK PLACE, according to Book 4 of Maps, page 40, records of Maricopa County, Arizona;

EXCEPT the South 30 feet thereof.

PARCEL NO. 4:

The portion of Lot 4, Block 2, PARK PLACE, according to Book 4 of Maps, page 40, records of Maricopa County, Arizona, more particularly described as follows:

COMMENCING at the Northwest corner of Lot 4, Block 2, PARK PLACE, a 1910 Arizona subdivision recorded in Book 4 of Maps, page 40, records of Maricopa County, Arizona;

THENCE South 00 degrees 24 minutes 57 seconds West, along the West line of said Lot 4, a distance of 194.19 feet to the POINT OF BEGINNING;

THENCE South 89 degrees 35 minutes 08 seconds East, departing said West line, a distance of 139.79 feet to a point on the West right-of-way line of 54th Avenue;

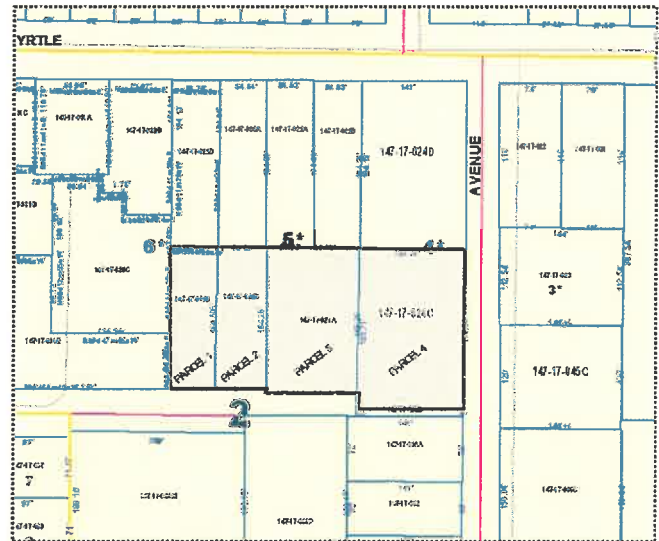
THENCE South 00 degrees 24 minutes 52 seconds West, along said right-of-way line, a distance of 185.83 feet to a point on the North line of a parcel described in Instrument No. 98-1053600, records of Maricopa County, Arizona;

THENCE North 89 degrees 32 minutes 11 seconds West, along said North line, a distance of 139.79 feet to a point on the West line of the aforementioned Lot 4;

THENCE North 00 degrees 24 minutes 57 seconds East, along said West line, a distance of 185.71 feet to the POINT OF BEGINNING.

PALMAIRE RIGHT-OF-WAY:

The north half of the Palmaire alignment has been preliminarily approved for abandonment and inclusion in the development site. Please contact Leon Graton, Property Agent, for details.



6.202.C.2 - *Intended Nature & Character of the Development*

As an infill development, the Palmaire subdivision is intended to build on the classic development patterns of earlier Glendale while maintaining current development design features to maximize the enjoyment of its residents.

The project site is located east of the original townsite for the City of Glendale, located on portions of lots 4, 5 and 6 of the Park Place subdivision, originally platted by the Glendale Land Company in 1910. The immediate neighborhood has a rather varied development pattern, with a mix of institutional and multi-family dwellings on surrounding properties, constructed at various times over the past 100 years. There is no dominating design vernacular for the area, resulting in a disjointed appearance that varies from lot to lot. Unfortunately, this surrounding context does not provide guidance to inform the design of a single-family typology. However, other examples were identified in the area to build the Palmaire design upon.



North across Myrtle Avenue, the Cook and White Addition subdivision provides a guiding framework for single-family development in the area. These homes are predominately a mix of post-war (late 40's to late 50's) structures, featuring modern designs on smaller lots. The lots are typically 50' feet in width, with homes towards the roadway. Front yards (to the property line) are about 17 to 20 feet in distance. The street features detached sidewalks, on-street parking, and about 28' feet of roadway surface. This design creates a compact roadway that acts to calm traffic, provides for sidewalk space that encourages pedestrian activity and establishes "eyes on the street" which improves neighborhood safety.

Another design feature of these homes is a small front porch, creating additional visual interest and a transition space from the public realm of the street to the private realm of the home. After review of these neighborhood conditions, it was determined that the character most appropriate for the Palmaire development is one that incorporates a similar design, with an intimate pedestrian streetscape, porches and homes that feature smaller front yards.

Additional studies of the surrounding context provide concepts that inform the materials used, size of the homes themselves as well as other detail ideas that the building elevations incorporate. The following pages provide the study results that informed the design program:



IMMEDIATE CONTEXT



COURTYARD / DEFENSIBLE SPACE



CONNECTION TO SIDEWALK



ODDITIES



Existing Site Context

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CRAFTSMAN

general proportions:
exclusive single story
roof type and features:
pitched roof
structural and face work materials:
high level of detail, small
punched window openings
spatial designation and floor plan:
strong connection to sidewalk



RANCH

general proportions:
mostly single story
roof type and features:
pitched roof, low level of detail
structural and face work materials:
punched window openings with some
larger
spatial designation and floor plan:
medium connection to sidewalk



SOUTHWEST

general proportions:
mostly single story
roof type and features:
mostly flat roof
structural and face work materials:
medium level of detail, few windows,
mix of punched and view
spatial designation and floor plan:
strong connection to sidewalk



HYBRID

general proportions:
mix between single/two-story
roof type and features:
mostly pitched roof
structural and face work materials:
medium level of detail, mostly
punched windows
spatial designation and floor plan:
medium connection to sidewalk



Adjacent Site Styles

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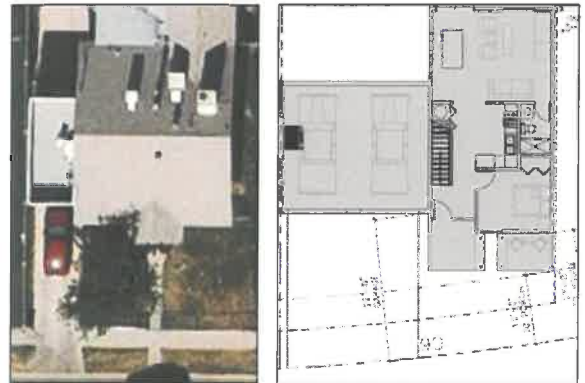
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By incorporating the appropriate elements from the surrounding context, the design of the subdivision and homes provides a contemporary neighborhood design in an established infill area that is currently lacking identifiable character. Designs will build off of the “classic” post-war residences described above to establish an appropriate appearance and feel for the subject site; an architectural theme we define as a “craftsman style” for this development.

Garages will be provided (a contemporary feature) but will be off-set to the side of the home. This element recalls the existing local neighborhood pattern where the original home either had a garage added later or a carport was enclosed to create a garage. These garages are usually located along side the home, set back some from the entryway. This type of design minimizes the garage’s appearance while reinforcing the front of the home as the primary focus. By variety in the color of the facades, the garages in Palmaire will appear to be in the background, reinforcing the quality of the front elevation of the residences.



Front porch elements are included as well. While porches were a common feature in pre-war homes, by the post-war period, the porch had been reduced in both size and functionality. Porches create a space that serves three functions. First, the space creates a transition between the public street area and the private interior of the dwelling, a sort of quasi-public space. Second, a functional sized porch provides a place to sit and expands the livable size of the home. Third, with the front yard as a functional space, the use of the porch provides “eyes on the street” which reduces undesirable activities. Porches are included in the design of the Palmaire homes to provide extra outdoor living space that will encourage closer neighbor relationships by creating an opportunity for residents to have casual contact with each other.



Upon completion, the Palmaire neighborhood is intended to create an intimate neighborhood where residents know each other and interact on a regular basis; the end result is a home ownership experience for residents that is obtainable, sustainable and enjoyable.

6.202.C.3 – Open Space

Purpose – Open space for Palmarie will serve three primary functions. First, the entire site is required to retain rain fall runoff in quantities as determined by the City. To accomplish this, the open space tracts will serve as locations for the retention of water during storm events.

The second purpose is to provide additional open space in a pocket-park type design for enjoyment by the residents. It is envisioned that the open space will consist primarily of grass play space and decorative landscaping at the perimeter. Habitat for Humanity would like to include a tot-lot type amenity in one of the tracts; however, as with their other developments, this element is only included if a donation is made. Since the site will be home to only 11 families, more intensive recreational space is not appropriate.

The final purpose, primarily of Tracts “A” and “D”, is to provide for a visually pleasing street appearance from 54th Avenue. The required setback area will be landscaped pursuant to the landscaping vision described herein.



Ownership – The open space areas will be owned by the Homeowner's Association.

Improvement- Improvements will consist of landscaping, turf in limited locations and, if available, play equipment of a scale appropriate for the subdivision.

Maintenance – All open space areas will be maintained by the Homeowner's Association.

6.202.C.4 – Development Phasing

The project is planned for a single-phase with infrastructure and home construction beginning promptly after approval of the subdivision plat.

6.202.C.5 – General Landscape Concept

Entry Feature: A more lush planting arrangement will be provided along 54th avenue, accenting the entry signage wall upgrade as indicated in the images in the attached materials. Preliminary design is showing palm trees (type to be determined) on either side to introduce the entry, and it is intended that there is a full compliment of native, low water planting along this edge.

Interior Streetscape: As discussed in the character section, Habitat for Humanity is interested in building the feeling of community in this development. To that end, the site plan provides, as much as possible, a tree-lined street to ease the edge and help the transition once on site from “vehicular” to “pedestrian.” Additional native plantings could occur at the planting strip between street curb and sidewalk. The materials have not been selected yet, but placement has been preliminarily determined to be between driveways, and between residential units. This helps accomplish Habitat for Humanity's goal of providing independent units, leading to a more prideful ownership position, and better overall community appeal for residents and neighbors alike.



Individual Lots: Because of the attention being paid to the first two landscape components, the desire is to keep the individual front yards developed with a minimum of materials. It is Habitat for Humanity's intention to have the front yards be tree-free, and suggest a minimum of six (6) shrubs selected from plantings on the city's approved list of low water use landscape plants. Ground cover treatment will be utilized for the remaining un-landscaped portions of the front yard, with an electric low water use drip system with timer provided for front yard landscaping. A secondary reasoning for the landscaping palette within the front yard area is a need to minimize long-term homeownership costs. Maintaining extensive landscaping increases costs for the homeowners; minimizing such costs is critical in the success of the individual residents as homeowners. It is envisioned that individual homeowners will, over time, populate the front yard areas with additional landscaping specimens customized to their individual taste and time availability for maintaining such additional materials.

6.202.C.6 – Outline of CC&R's

While not drafted as of yet, the Conditions, Covenants and Restrictions (CC&R's) will contain typical language regarding structure of the Homeowner's Association, requirements for paying dues to the association for maintenance of the common elements (including landscaping tracts and exterior fence) as well as rules regarding the appearance of the development. Homes will be restricted for residential uses only, open storage and outdoor use will be regulated, as well as requirements for trash pickup as approved by the Solid Waste Department. One unique requirement will be a restriction on the conversion of garage space into living space of any sort; this is due to the shared wall with the adjoining garage and the need to preserve available parking spaces for residents.

6.202.C.7 – Amended Development Standards

The R1-4 zoning district states that the “primary intent of this district is to encourage the establishment of functional and attractively designed patio home developments. The subdivision and housing product shall be designed for rear yard privacy and useable private open space”. Key to meeting this intent is establishing a subdivision design that maximizes rear yard space while creating an attractive and functional streetscape.

To meet the intent of the Zoning Ordinance, the design challenge for Habitat for Humanity was addressing the narrow width of the subject site. With only about 185' feet of frontage along 54th Avenue, lot depth is a significant challenge to achieve while providing for public street right-of-way widths. The solution is to create a streetscape that is pedestrian oriented. As discussed prior, the design is intended to create a narrow street section, provide detached sidewalks and to incorporate front porches which will establish a visually pleasing experience for the public while permitting the homes to be closer to the road. This forward focused front façade not only is similar to existing development patterns, but also meets the design goal to create rear yards of usable size and shape. The following table illustrates the typical R1-4 standards and the proposed Palmaire standards, which are modifications to permit larger rear yards by moving the dwellings closer to the street.

<u>Standard</u>	<u>R1-4</u>	<u>Proposed</u>
<i>Minimum Lot Area</i>	4,000 SqFt	3,161 SqFt Min – 3,481 SqFt Average
<i>Minimum Lot Width</i>	40 Feet	49 Feet
<i>Minimum Lot Depth</i>	80 Feet	61 Feet
<i>Minimum Setback</i>		
Front – Porch	N/A	9 Feet Min – 10 Foot Average
Front – Living	15 Feet	15 Feet
Front – Garage	20 Feet	20 Feet
Rear	15 Feet	12 Feet, 6 Inches – Min - 17 Feet Average
Side	0/10 Feet	0/5 Feet
<i>Maximum Height</i>	30 Feet	25 Feet
<i>Maximum Lot Coverage</i>	45%	45%

The intent of the design is to feature homes that are functional in space layout, providing maximum utility while reducing construction costs. The Habitat for Humanity model requires extensive inputs of volunteer labor and materials; providing floor plans that are easily constructed while establishing a high quality visual appearance is a critical element in the success of their developments. Deviations are rationalized as follows:

Lot Area, Width and Depth – To create usable lots, the lot depth is constrained by the width of the entire site (185'). Removing the right-of-way of 50' feet retains about 135' feet of lot depth to disperse amongst two (2) lots (one on either side of the roadway), or about 67' feet total. An additional design issue is the extra depth provided in the rear of the subject property (along the Palmaire alignment). To distribute lot size more equally, the design features a curve in the roadway which permits more depth for each lot over the entire development while enhancing visual interest. Since there is no additional depth available to deepen the lots, the only way to add size would be to lengthen the lot fronts, which would exceed the 49' feet in the current plan (which already exceeds the typical R1-4 standard by 22%). The result of this would be very large, long lot frontages and an unacceptable loss of homesites, rendering the development impractical. Additionally, the homes would seem “out of scale” to the lots with large side yards (which are unsuitable for usable open space). The proposed lot area, width and depth standards are appropriate to the Palmaire development given the unique nature of the subject site and the desire to create a pedestrian oriented subdivision design for the neighborhood.

Setbacks – As with the lot standards, the application of the standard setback requirements would not be appropriate for the Palmaire development. Due to the need to balance the lot sizes and to address the unique shape of the rear of the subject site, the setbacks require some flexibility to accommodate this infill development. All standards attempt to reach an “average” that meets or exceeds the “typical” R1-4 standard while recognizing the specific unique circumstances of the entire development site.

Since the streetscape is pedestrian oriented, the street cross section includes a detached sidewalk (see the Cross Section discussion section for details). This establishes the appearance of larger front yards as the portion of the public right-of-way between the sidewalk and the curb appears to be part of the front yard for the home. By including landscaping in this space, that appearance is reinforced. Therefore, smaller setbacks from the right-of-way are appropriate. Additionally, the placement of the home closer to the street creates more rear yard space, a key intent of the underlying zoning district.

Front Yard - The closest living space within the development to the edge of right-of-way (not edge of curb, which is 9 feet beyond) is 15.25 feet on Lot 9. A majority of the lots provide in excess of 17 feet for the front yard setback to livable space, exceeding the underlying R1-4 standard of 15 feet. Note that there is an additional 9 feet to the back of curb, creating the visual appearance of a 26 foot setback from the roadway to the living space. Combined with the streetscape and roadway bend, the appearance of all homes will be appropriate to similar R1-4 developments and will compliment the craftsman style of the architectural design. No deviation from the 15 foot setback standard is necessary.

Porch – Because the porches need to be closer to the sidewalk to function as interaction opportunity space, it is appropriate that these features encroach into the required front yard area as they are not enclosed space nor habitable. Lot 9 is the most constricted lot in the development due to the “L” shape of its southern boundry, so only 9.25 feet is available to provide between the porch edge and the edge of right-of-way. Most of the other lots provide 11 feet or greater, with an average throughout the development of about 10 feet. These projections will enhance the facades of the homes and increase the visual interest from the street and are an appropriate addition to the design of the dwellings. Since the underlying R1-4 does not contain a specific standard for such elements, the Palmaire PRD provides a 9 foot minimum standard with a required average of 10 feet for porches.

Rear Yard – As with the other bulk standards, the development site’s odd shape requires some minor modification to the typical R1-4 requirement of 15’ feet for a typical rear yard setback. Overall, the entire subdivision provides an average of 17 feet for rear yards; however, with the most constricted lots (Lot 1, Lot 2, Lot 9 and Lot 10) there is a need to reduce the standard to 12.5 feet, which addresses the “L”- shape of Lot 9 in particular and the curvature feature required for the roadway design. Being that all but four (4) of the lots can provide the R1-4 minimum of 15’ feet or greater of rear yard, it is appropriate to permit minor deviation for the unique lots within Palmaire Courts.

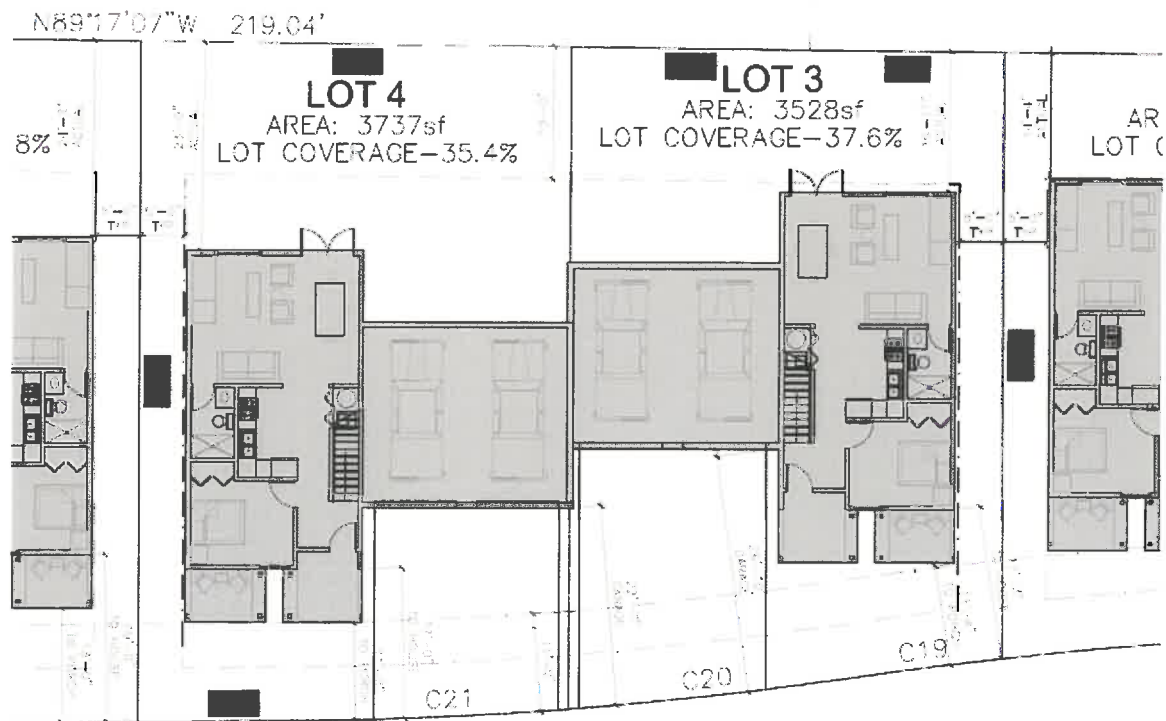
Side Yard – Typically, R1-4 permits a 0’ foot setback, subject to a 10’ foot separation between “buildings”. In the case of Palmaire, there is 10’ feet provided between livable spaces (the homes). The garages (non-habitable space) share a common wall, which is at 0’ feet. This design permits a savings in material for the construction of the residence, a reduction in the long-term maintenance costs for homeowners and is necessary to accommodate a two (2) car garage, a feature that preserves property values in the long run. The Palmaire PRD proposes a 10’ foot separation between “habitable space” and a 0’ foot separation between “non-habitable space”. The CC&R’s will include a restriction on conversion of garage space into habitable space of any type.

Height – Bulk regulations are intended to provide access to sufficient light and air, amongst other things. With the proposed deviations from some of the typical R1-4 standard setbacks, it is appropriate to reduce the allowable height in Palmaire. In lieu of the typical 30’ feet of height permitted, Habitat for Humanity will restrict the permissible height to no greater than 25’ feet for the Palmaire development, ensuring lower scaled homes more appropriately scaled to the streetscape concept proposed and to reinforce the craftsman style of architecture utilized in the home designs.

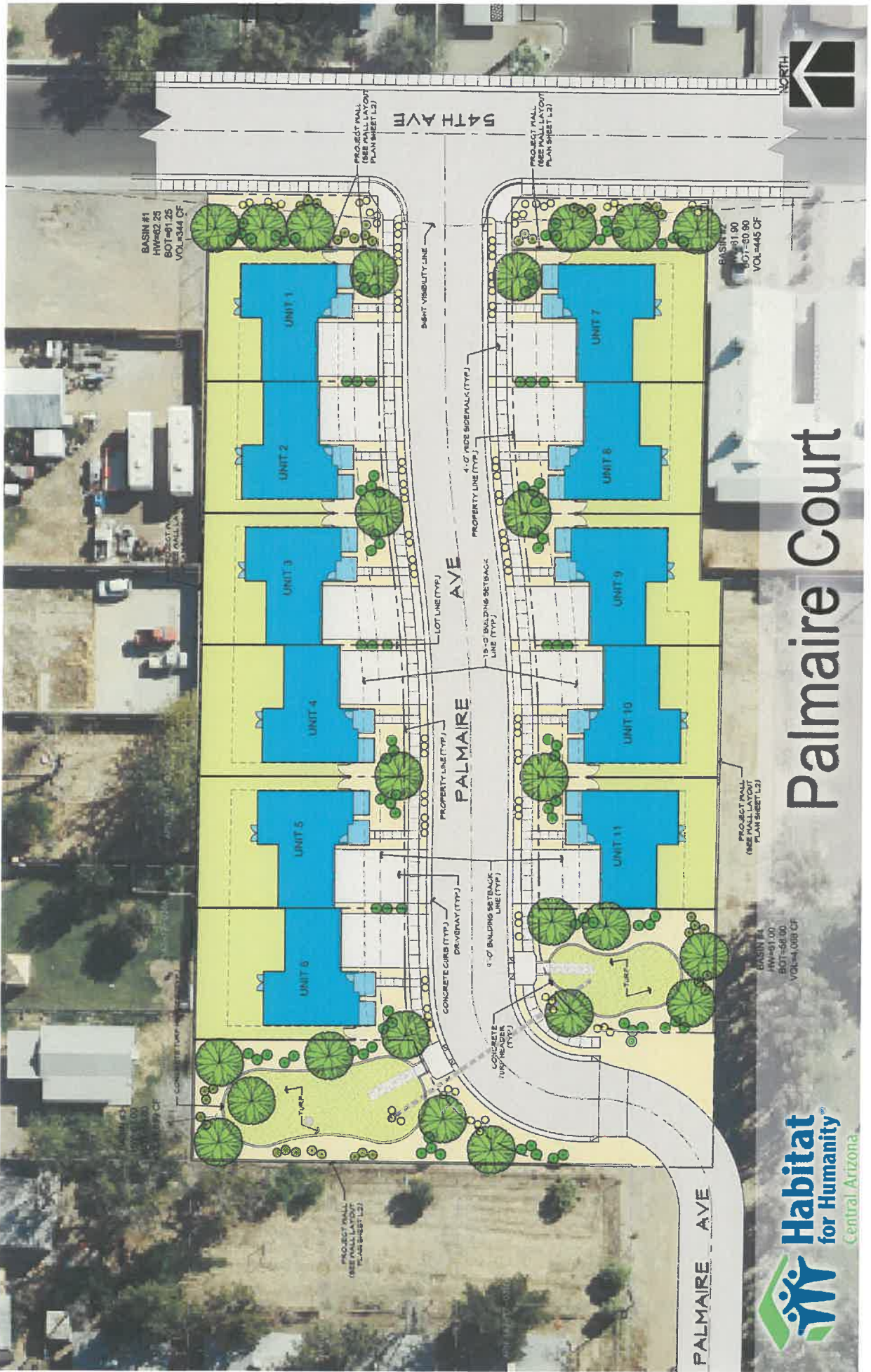
The above listed standards are the only deviations from standard R1-4 development requirements required to facilitate the development of the Palmaire neighborhood. The proposed design meets all other requirements of the City of Glendale Zoning Ordinance, not limited to parking provision, landscaping/open space and street design.

6.202.C.8 - Typical Lot Layout

The typical configuration for the homes is shown on the following page. In overview, the intended placement for the dwellings is in closer proximity to the roadway, while maintaining sufficient space for the provision of additional parking in the front driveway (20' foot minimum). Proximity to the roadway is ideal as this creates three benefits for the residents. One, the homes maximize rear private yard space by reducing the front yard (which is typically not utilized for recreational activities). Two, a smaller front yard reduces maintenance time and costs for the homeowner. Third, and most importantly, the orientation towards the street reinforces the character goal of establishing a traditional residential street with a new urbanist design approach; in essence, establishing a streetscape that is at a human scale and is pedestrian oriented, as discussed in greater length in Section 6.202.C.2.







Palmaire Court

6.202.C.9 – Relationship to Surrounding Properties

The surrounding context for the Palmaire site is one of various development typologies established at various periods over the past 50+ years.

To the north exist 5 lots developed with a mix of single family and multi family structures. These buildings are located towards the front of the lots, providing for 75 feet or more of separation from living space to the property line.

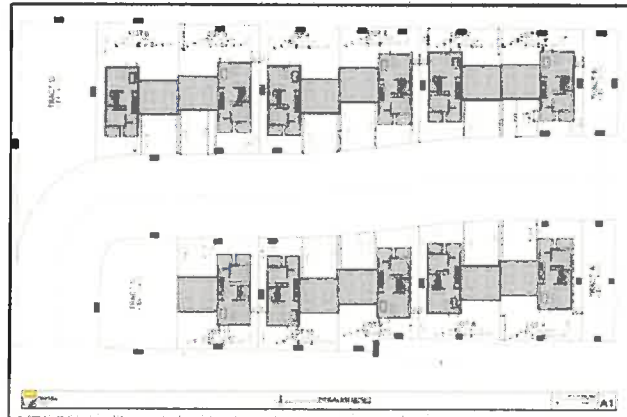
To the west is an apartment complex, with approximately 45 feet of separation between the units and the property line.

Directly south are two institutional uses, both with large open areas between the subject site and the primary buildings. A duplex structure is located at the southeast corner of the site.

Across 54th Avenue to the east are other small apartment complexes.

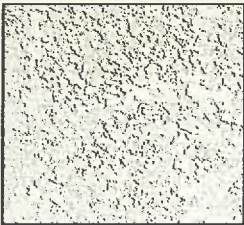


Because of the existing development patterns, the design program establishes a cul-de-sac situation (although an access gate for garbage trucks and emergency vehicle ingress will be located at the southwest corner of the site in lieu of a turn around) to provide a neighborhood within the context of a larger community. The lot configurations places private, quieter spaces along the exterior lot line at the north and south, providing complementary space to the adjoining neighbors. This design of rear yards against rear yard spaces maintains a compatible relationship between the new residences and the existing dwellings to the north and south. Open space will be located between the west apartment units and the nearest dwelling, reinforcing the compatibility of the Palmaire subdivision within the context of the larger neighborhood.

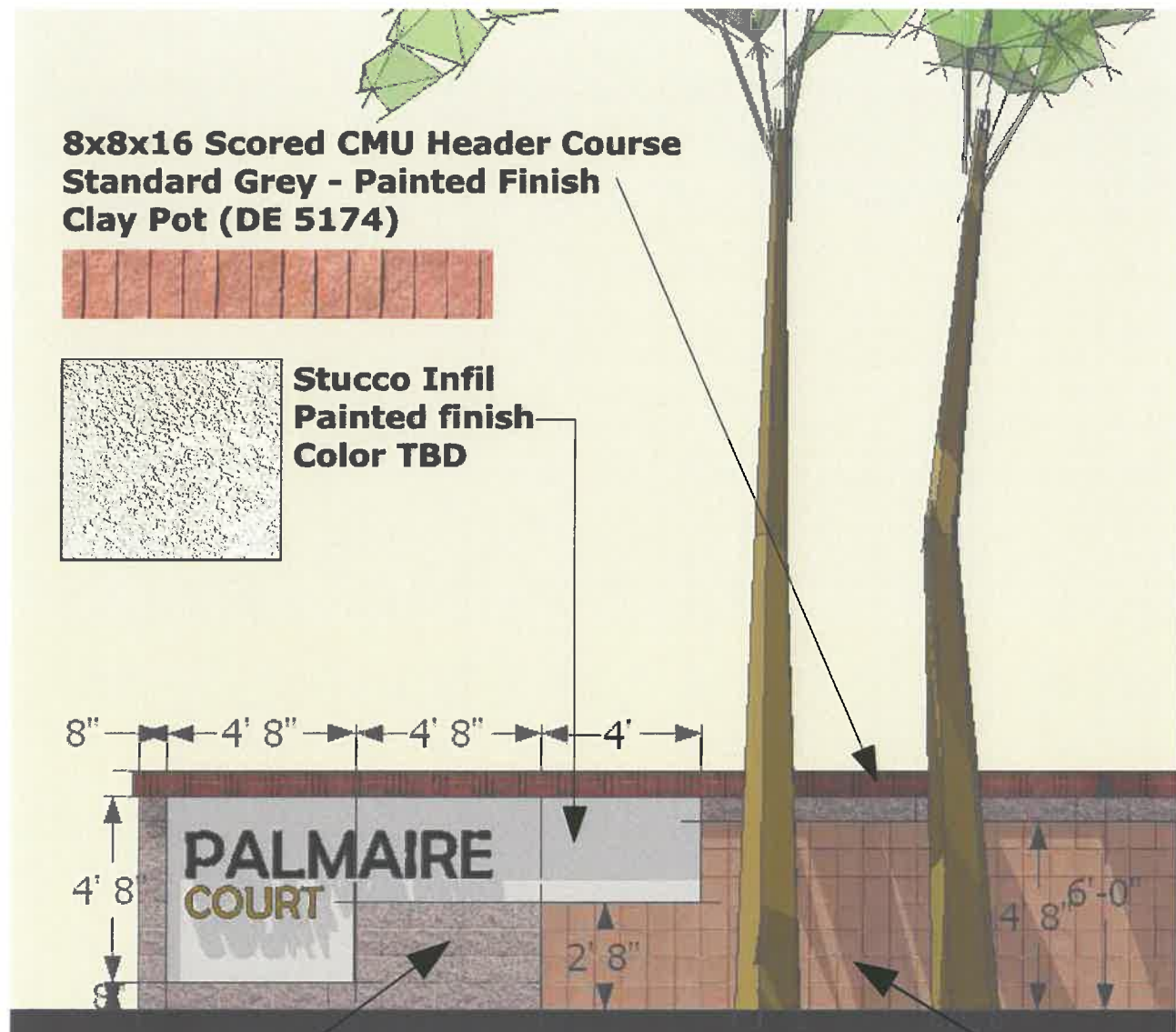


Overall, the Palmaire subdivision will introduce new residential uses in an area of existing residential uses, will be a compatible land use, and will benefit its surrounding neighbors.

**8x8x16 Scored CMU Header Course
Standard Grey - Painted Finish
Clay Pot (DE 5174)**



**Stucco Infil
Painted finish
Color TBD**



**8x8x16 Scored Block (8x8 profile)
Standard Grey - Painted Finish
Field of Wheat (DE 5262)**



**8x8x16 Split-Faced Block
Standard Grey - Painted Finish
Trail Dust (DE6123)**



Perimeter Wall Design - Entry Feature

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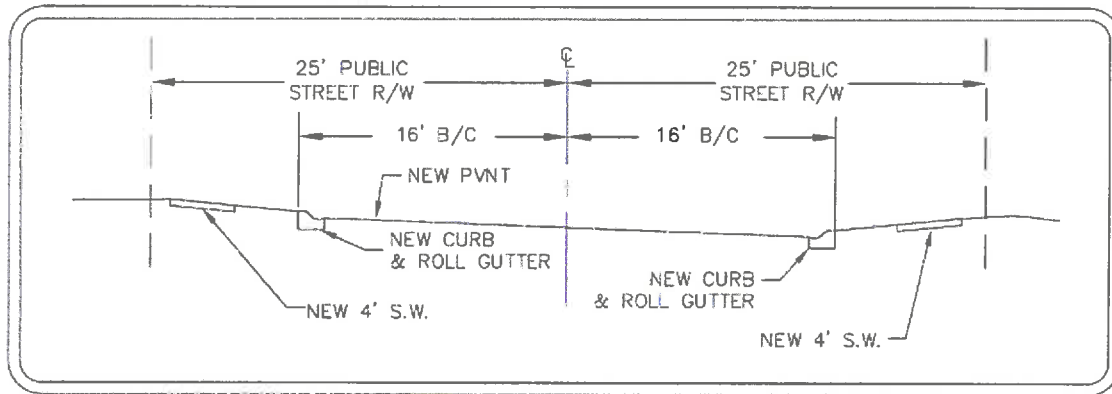
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6.202.C.11 - Street Cross-Section

Palmaire will be a public street; however it will terminate into an access gate at the western property line. This gate will be for trash service vehicle access to the site and for use by emergency vehicles only. This limitation on movement is intended to minimize impacts on the interior of the larger area neighborhood that would be created if traffic could move from 54th Avenue over towards 54th Drive and points beyond.

The cross section will provide for 32 feet of paved surface within a 50 foot public right-of-way. Sidewalks will be detached, providing space for additional landscaping and a more pedestrian oriented streetscape, as illustrated below:

TYPICAL STREET SECTION B-B



6.202.C.12 - Home Design Criteria

The following pages provide an overview of the design of the homes, including floor plans and conceptual elevations. The final floor plan of the dwellings, their elevations and color palette will be in general conformance with the illustrations provided.

Due to the necessity for Habitat for Humanity to utilize as many donated materials as possible, finishing materials may be somewhat varied from what is proposed currently. Homes may feature tile roofs or asphalt shingles; this is dependent upon availability of either material from various sources.

Materials utilized in the finish of the homes will be similar to what is shown in the following pages.

Asphalt Shingles

Certainteed "Landmark" or equal

(Conc. tiles will be used if donors available; colors to be compatible)



"Terra Cotta"



"Painted Desert"



"Heather Blend"

Accent / Trim Color - Painted wood

DE5526 Dusky Moon

DEC755 Cocoa

DEC702 Antique Rose

Painted Wood Siding - Base color

DEC702 Antique Rose

DEC745 Quicksand

DE6227 Muslin

Painted Stucco - Base color

DEC717 Baked Potato

DEC750 Bison Beige

DE6389 Fallen Rock

Recessed Painted Garages - Typical at each unit

DEC755 Cocoa



Color | Exterior Materials - 3 options

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Typical Front Elevation

Scale: 1/8" = 1'-0"

A
3



Typical Side Elevation

Scale: 1/8" = 1'-0"

B
3



Building Elevations

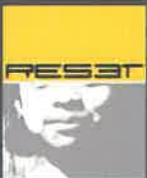
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Building Elevations

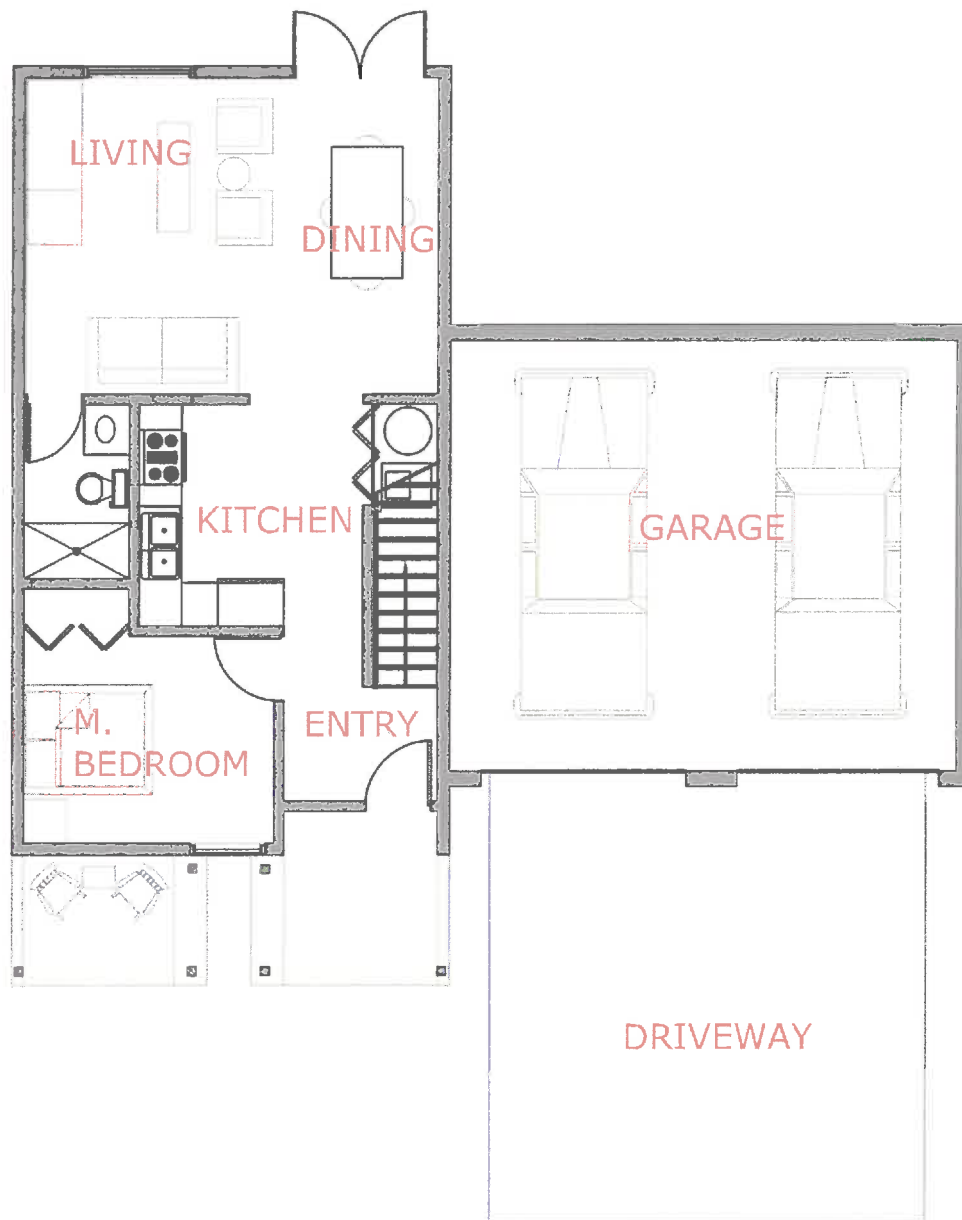
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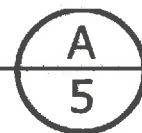
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July 27, 2008



Ground Floor Plan

Scale: 1/8" = 1'-0"



Ground Floor Plan

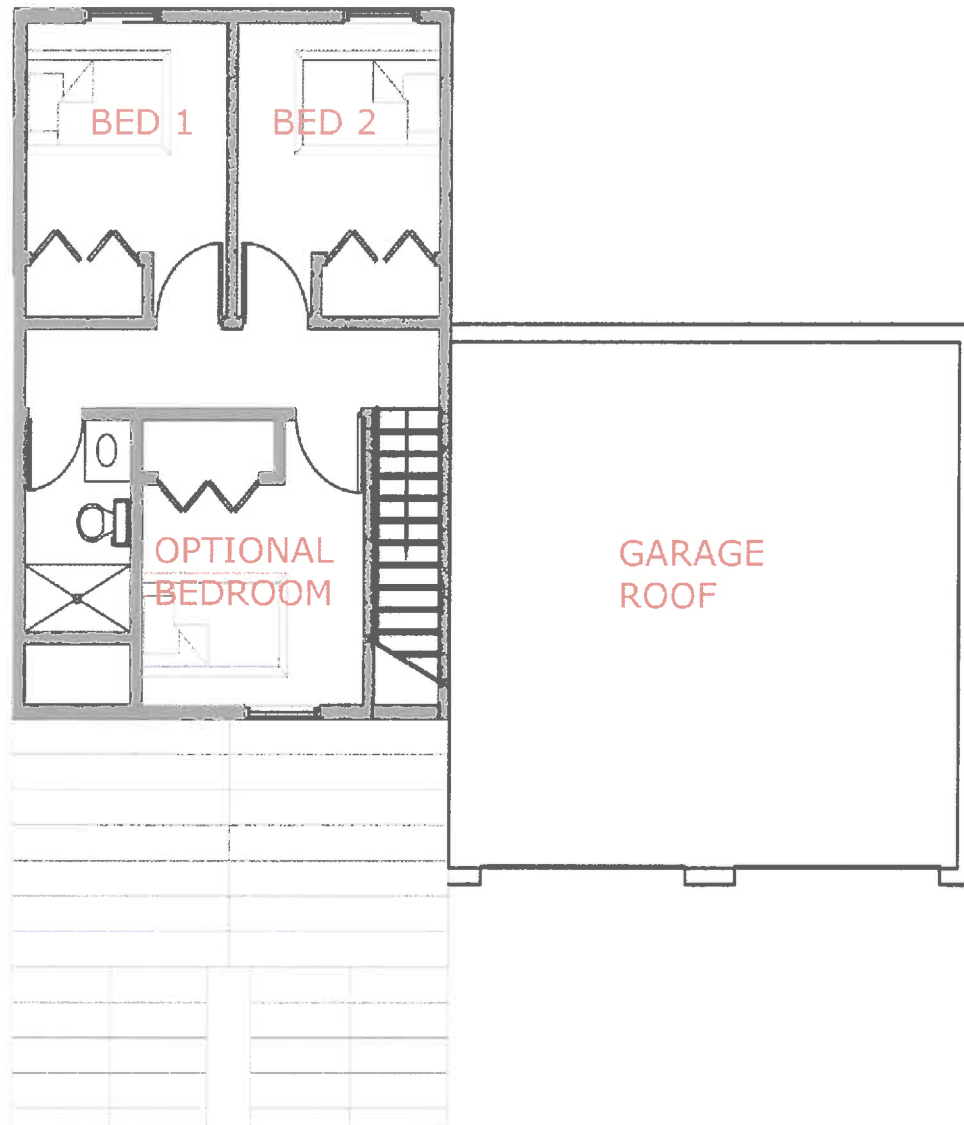
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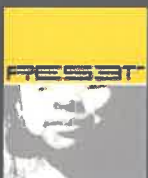
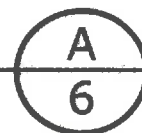
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Second Floor Plan

Scale: 1/8" = 1'-0"



Second Floor Plan

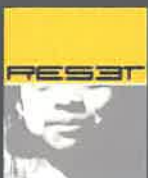
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Renderings

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6.202.C.13 – On & Off Site Drainage Provision

As an infill location, there are no established cross-site water flows since the adjacent properties are currently developed and any runoff produced on those properties flows away from this site. However, as with all development, there are drainage issues to resolve when transitioning from a vacant site with permeable surfaces to a built environment of residences, roadway and assorted semi- and non-permeable surfaces.

Mitigation of runoff from the development will be addressed via storm drains and retention basins that will comply with City of Glendale Engineering Standards. The development will retain the required 100-year, 2-hour storm event and supply one foot of freeboard, as required by the City of Glendale. All final engineering plans will be reviewed and approved by the city to ensure compliance with applicable rules and regulations.

6.205 – Findings

As required by the Zoning Ordinance (Section 6.205), the Planning Commission must find that this proposed PRD complies with the specific items listed. Habitat for Humanity believes that this request is in conformance with the requirements of the PRD district, as demonstrated below.

6.205.a. General Plan – This request is in concurrence with amendment of the land use classification of the General Plan Land Use Map to identify the subject property as appropriate for a density range between 5 to 8 dwelling units per acre. With this modification, the proposed Palmaire Courts development, at about 7.74 dwelling units per acre, will be in conformance with the Land Use Map of the General Plan.

Additionally, this request is supported by various elements of the General Plan, including the Housing Element. Goal 2 states that the City should “provide the citizens of Glendale choice from a wide variety of housing types”. Goal 3 encourages the City to “promote quality, affordable housing for every citizen of the City”. Support for this proposal is also found in the Redevelopment Element, which states that the City should “consider infill development a top priority” (Goal 2). Approval of this request will forward these goals by providing a housing opportunity that is affordable, of high quality and is located in an infill location.

6.205.b. Compatibility – This request will provide for development of residential uses that are compatible in intensity, scale and use with the surrounding properties. The design of the subdivision complements the existing development pattern of the surrounding properties and will provide for a harmonious relationship between existing land uses and the new residences.

6.205.c. Conformance to Subdivision Design Expectations – The proposed subdivision meets or exceeds the City of Glendale subdivision design requirements. The Palmaire development features a design that provides for a modern pedestrian oriented streetscape while ensuring adequate site access, functional form for life

safety vehicles and utility services equipment, as well as conformance to specific engineering design standards.

6.205.d. Creation of Quality Living Environment & Lifestyles – By design, the proposed development establishes a quiet, neighborhood scaled street that features design elements, such as front porches, that will assist in creating an environment for homeowner's in Palmaire to interact on a daily basis. These interactions will help to build relationships between residents, create a functional neighborhood feel and provide a homeownership opportunity that they otherwise might not be able to access.

6.205.e. Amenities Adequate and Appropriate – The development features a combination of semi-private open space (front porches), private open space (rear yard areas) and quasi-public open space (open space tracts), each providing a unique outdoor experience. The small scale of the development and the center city infill location supports the proposed level of amenities. This infill location is located close to other public facilities, such as Sands Park, that will supplement the recreational opportunities for residents. Given that the development features only eleven (11) dwellings and is an infill development site, the level of open space provided is adequate and appropriate for the future residents.

6.205.f. Home Product is Consistent with Project Character – Home elevations feature design elements that build upon the existing development elements found in the surrounding neighborhoods located near the subject site. The desired character of the Palmaire development, including its pedestrian scale, is reinforced by the conceptual elevations provided. The proposed design of the housing product support the desired project character as described in this application.